



# **N62 Slievenamon Road Phase 2 Road Improvement Scheme**

Preliminary Temporary Traffic Management Plan

July 2022



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# 1 Introduction

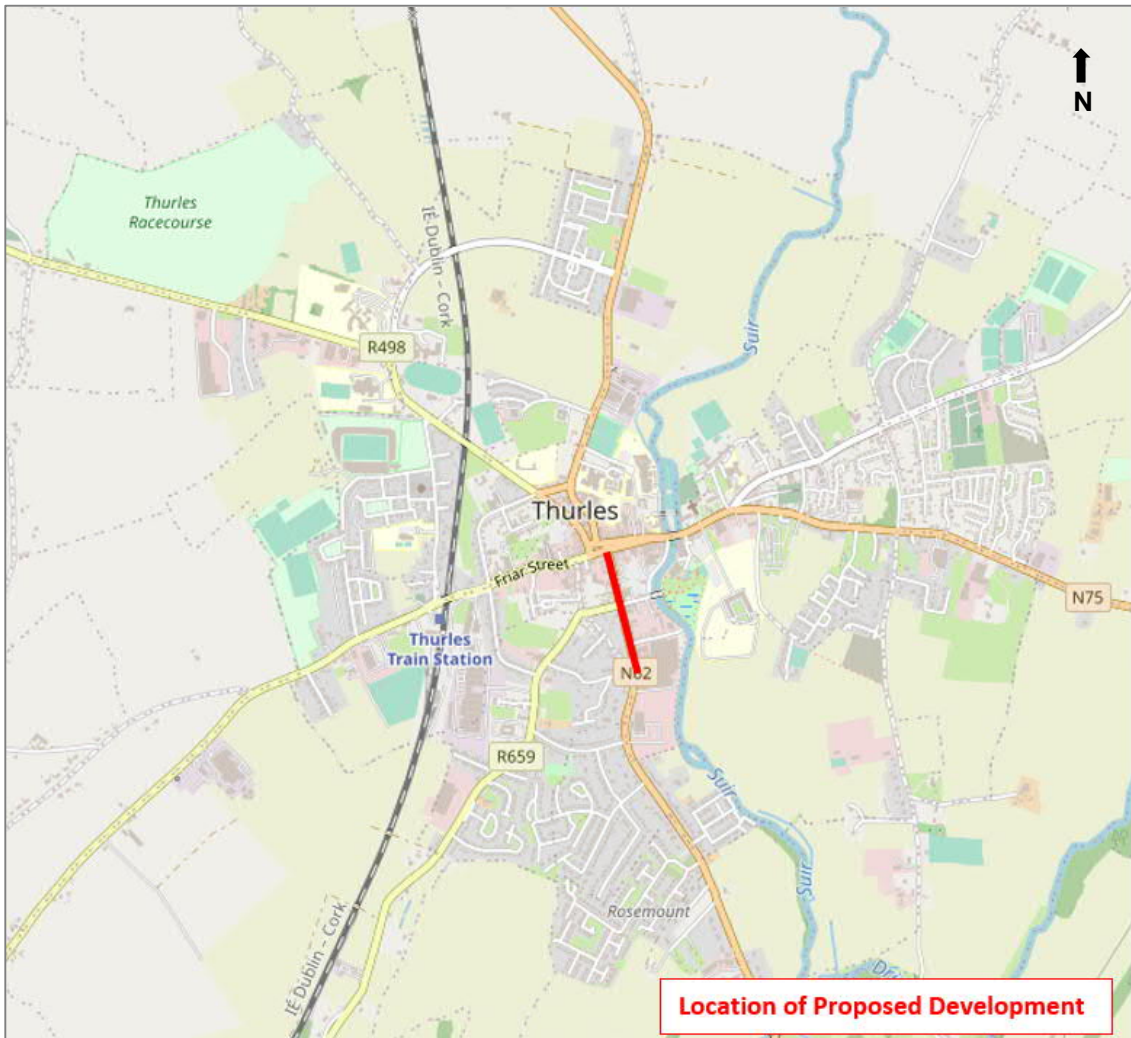
## 1.1 Overview

The proposed scheme is located on the N62 Slievenamon Road, Thurles, Co. Tipperary. The road length under consideration extends from the Slievenamon Road Roundabout at Thurles Shopping Centre at the southern extent to Liberty Square at the northern extent. The scheme is approximately 450m in length.

The N62 scheme has been assessed under the following TII Road Safety Publications.

- *AM-STY-06044 Road Safety Inspection (December 2017)*

This report provides the preferred treatment option including a programme for implementation. A scheme location map is provided in Figure 1.1.



**Figure 1.1 Scheme Location Map**

The N62 Slievenamon Road forms part of the main access into Thurles from the south linking the M8 Cork-Dublin motorway to the town centre. The nearest traffic counter is located between



Thurles and M8 Junction 6 Thurles, Knockroe (TMU N62 050.0 S). Based on 2019 data, the AADT is approximately 8,650 with 5.0% HGVs.

The site extends from Liberty Square – the main commercial centre of the town – at its northern end, to an unnamed 5-leg roundabout located at Thurles Shopping Centre at its southern end. The street is bisected by the N62/R659 Fianna Road / Thomond Road Junction. The buildings to the south of the N62/R659 junction are primarily residential comprising semi-detached and terrace properties with intermittent commercial units including a service station. The buildings to the north of the N62/R659 junction are primarily commercial with “over the shop” residential units. The street has a posted speed limit of 50kph. The extents of the proposed N62 Slievenamon Road Phase 2 scheme are shown in Figure 1.2.

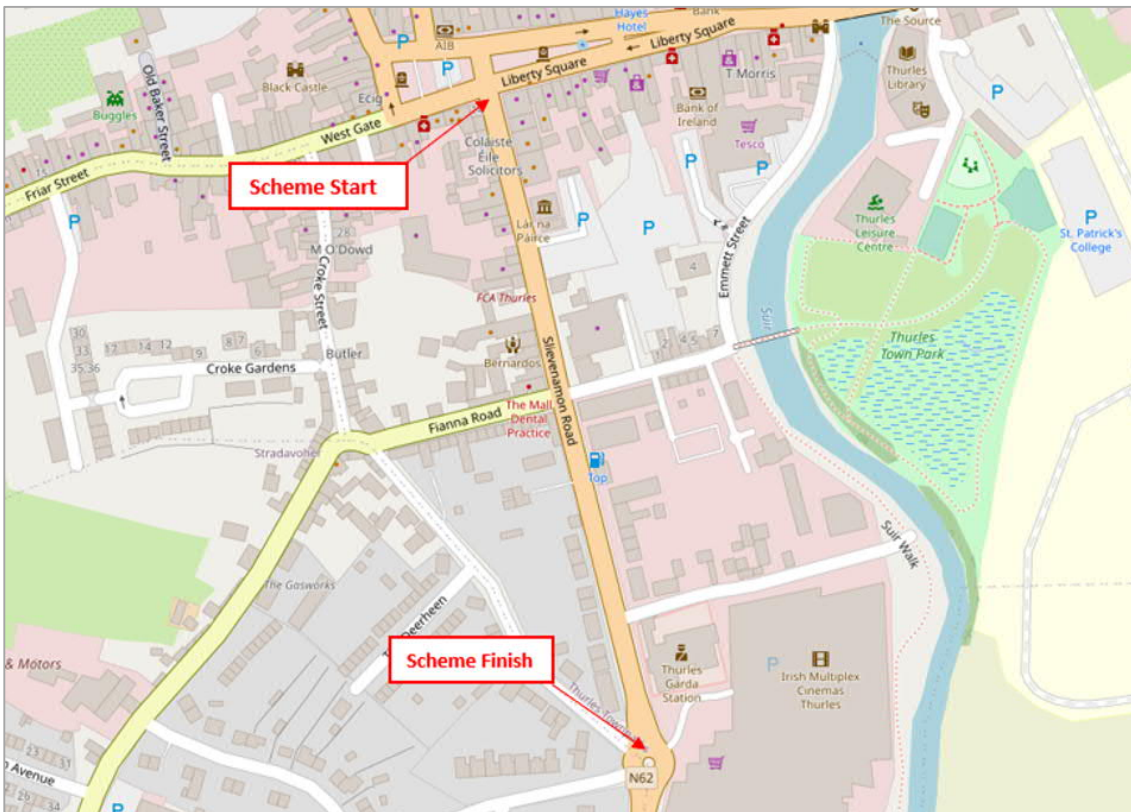


Figure 1.2 N62 Slievenamon Road, Thurles

## 2 Traffic Management

### 2.1 Introduction

The Contractor must ensure that a suitable Traffic Management Plan is prepared and implemented to ensure that the public and other users of the site are not put at risk during the works.

The design of all traffic management is to be forwarded to the PSDP for design coordination purposes. At a minimum, this shall include layout plans/drawings, together with a site-specific design risk assessment relating to the traffic management works area.

The Contractor shall nominate and appoint a fully trained and experienced person with direct responsibility for the implementation of the traffic management plan during construction including installing, maintaining, modifying and removing the temporary traffic management arrangements.

The Traffic Management Plan shall comply with the requirements of “Chapter 8 of the Traffic Signs Manual 2019 & the Temporary Traffic Management Document Suite

The Contractor shall also take account of the “Guidance for the Control and Management of Traffic at Road Works” Second Edition issued by the Department of Transport (2010) in the implementation of the plan and the “Guidelines for Managing Openings in Public Roads” Second Edition issued by the Department of Transport, Tourism and Sport (2017).

The traffic management plan must at a minimum include the following:

- Site Traffic and Parking
- Deliveries
- Storage of Materials
- Safe Access and Egress
- Other Road Users, particularly cyclists and pedestrians
- Local Residents
- Existing Entrances
- Emergency Services
- Working Hours
- Safety & Security
- Speed Limits
- Restrictions to Traffic
- Safe Working Widths
- Safety Zones

### 2.2 Site Traffic and Parking

The Contractor shall designate an area for staff parking. This area shall:

- Allow safe access and egress of vehicles and personnel,
- Not obstruct other access or sightlines from same,
- Not be located within designated safety zones,

- Be clearly delineated with barriers and signs in accordance with the aforementioned Standards should it be located on a public road.

### 2.3 Construction Traffic

The Contractor shall designate an area for collection / drop-off of materials in the plan of Works. This area shall:

- Allow safe access and egress of vehicles and personnel,
- Not obstruct other access or sightlines from same,
- Not be located within designated safety zones,
- Be located adjacent to where the materials shall be stored to minimise double-handling and traffic movements on site.

### 2.4 Site Access and Egress

The Contractor shall ensure that the Works are undertaken in a manner which facilitates the safe access and egress of vehicles, pedestrians and cyclists. Furthermore, the Contractor shall ensure that the Works are undertaken in a manner which facilitates the safe access and egress of staff and visitors for the duration of Works, including suppliers of materials to the Works.

### 2.5 Diversions and Road Closures

Temporary road closures may be required where the Contractor can demonstrate, based on a risk assessment carried out by the Contractor, to the Client that such a closure is necessary, for the safe execution of the works.

Road closures and diversions shall be agreed in advance with the Local Authority and provision made to allow adequate timing for the legislative process of obtaining the closure in accordance with the procedure under **Section 75 of the Roads Act 1993, Article 12 of the Road Regulations 1994** and DTTAS **Guidelines for Managing Openings in Public Roads 2017**.

The following is required at a minimum:

- Two public notices required - one at least 14 days before proposed closure (intention), one at least 7 days before closure (decision).
- The Superintendent of the Garda Síochána within whose district the road is situated must also receive the above notice in writing.
- Manager's Orders are required for both Intention and Decision.
- Notice of intention must invite objections, which must be considered prior to a final decision being made.
- Both the Notice of Intention and Notice of Decision must state:
  - The Road to be closed
  - The Period of the closure
  - The Reason for the closure
  - The Alternative route(s) available

Classification of the roads on a diversion route shall be similar to the road to be closed as possible. The shortest route possible shall be chosen and appropriate road diversion signage put in place. The period of any such closure shall be the minimum to facilitate the works. The period of any such closure shall be the minimum to facilitate the works.

## 2.6 Emergency Services

The Contractor shall ensure that the local Gardaí are notified of the location, duration and nature of the Works.

Where a Stop/Go system is in operation, competent personnel shall be operating same and shall give priority to emergency vehicles to ensure they are not unduly delayed.

## 2.7 Vulnerable Road Users

Cyclist and pedestrian movements and other such movements must be expected by the Contractor and must be fully taken into account and safely catered for in the planning and execution of the Works.

The Contractor shall ensure that cyclists and pedestrians are fully considered in the preparation of the Traffic Management Plan and that the Works are safely undertaken in full consideration of same.

## 2.8 Working Hours

The Contractor/PSCS is to liaise with Tipperary County Council regarding restrictions on working hours and must comply with the relevant stipulations detailed in Works contract.

The road works might need to be carried out in the vicinity of schools or other sensitive sites. Special consideration needs to be given to such sites and might require restricting certain hours of work or introducing exclusion zones for works. These need to be liaised with the relevant parties in advance, details of the arrangement are expected to vary between different parties.

As is good practice, no person must work on his or her own on the site at any time. Working hours should be restricted to daylight hours unless specifically required by relevant Authorities.

## 2.9 Safety and Security

The Contractor shall ensure that the Works are secure at all times. All areas of the site which are under investigation or are storing materials shall be secured.

## 2.10 Traffic Signs

Traffic signs and other apparatus for the control of traffic must conform to the requirements of Chapter 8 of the Traffic Signs Manual encapsulated in the **W-I-D-E** approach to signing at road works:

- **Warn** the user of the forthcoming works
- **Inform** the road user of the characteristics (e.g. road narrows, traffic control ahead etc.)
- **Direct** the road user safely through the site by means of arrows, cones etc
- **End** clearly sign the end of the road works site

## 2.11 Variable Message Signs

Variable message signs may be erected to advise motorists of likely delays.

## **2.12 Public Notices**

The Contractor shall notify, the local authority, local and national radio stations, local newspapers and AA Roadwatch of the commencement and duration of the proposed temporary traffic management works.

## **2.13 Carriageway and Verge Maintenance**

The Contractor shall be responsible for maintaining the running traffic carriageway, any pedestrian routes and all carriageway verges adjacent to and in the vicinity of the Works in a clean and safe condition at all times.

## **2.14 Bus Operators**

Suitable locations should be agreed with the Gardaí and bus service operators (including school buses) for temporary bus stops where existing facilities may be affected by the works. Specifically, the bus stop outside Thurles Garda Station on the N62 may be impacted if road closures are required during the works; alternative arrangements to be agreed with TFI Local Link for Route 896 during the works.

## 3 Minimum Requirements for Traffic Management

### 3.1 Road Classification

Requirements for traffic management to be implemented in accordance with Temporary Traffic Management Design Guidance, published by the Department of Transport, Tourism and Sport 2019. The scheme falls under a Level 1 classification, encompassing “*Low Speed and Urban areas using geometric standards set out in DMURS*”, as per Table 0.4.3.1.2 of Temporary Traffic Management Design Guidance.

### 3.2 Road Openings

Road opening works are generally carried out using Static TTM methods described in Section 0.5 of Temporary Traffic Management Design Guidance. The TTM will be static but the TTM Designer must take account of how the works will be phased and the effect the road geometry will have on the layout:

- Due to the length of the site, the works will need to be phased to ensure that appropriate shuttle lengths are implemented.
- The TTM will need to be moved so the TTM Designer should assess road geometry throughout the required works area. The location of advanced warning signs, tapers etc. should be specified for each phase to ensure visibility requirements are maintained and that TTM is located appropriately.
- Warning signage for uneven surfaces, temporary surfaces, ramps etc. should be specified where required in addition to the advanced warning signage for traffic control. Locations should be specified such that visibility to all signage is maintained.
- The TTMP should include layouts for any junctions encountered.

The job information pack for the works should include method statements for switching between phases and movement of the TTM as the works progress.

### 3.3 Arrangements for One-Way Shuttle Working

Where one-way shuttle systems are required to facilitate the works, the one-way shuttle systems shall be in accordance with Section 0.5 of the Temporary Traffic Management Design Guidance. Design of these systems are to account for the specific requirements of Part 1 of Temporary Traffic Management Design Guidance, relevant to low-speed urban areas.

All works are to be of a maximum length of 200m. For areas greater than 200m in length, works are to be phased such that individual works areas are less than 200m in length to ensure waiting time of traffic is minimised. Where the required length is above the 200m maximum limit, consultation shall take place and permission sought in advance from Tipperary County Council.

A one-way shuttle system should not be operated during peak hours (all weekdays between 07:30 and 09:00, and 16:30 and 18:00).

The one-way shuttle system shall be designed to operate on the basis of a maximum time delay per vehicle of 10 minutes and a maximum queue length of 50 vehicles. If unacceptable traffic congestion disruption occurs as defined above, or where queuing traffic affects the operation of

another junction, the shuttle working is to be removed forthwith and an alternative method employed. The ALL STOP period shall not exceed 3 minutes.

Before implementing a one-way shuttle system, the Contractor must first obtain the necessary consents and detailed traffic management plan approvals (including management of pedestrians) from the Local Authority and An Garda Síochána.

At all times when one-way shuttle working is in operation during Contractor’s working hours, a sufficient number of suitable operatives are to be in attendance solely for traffic management duties. In addition, each junction shall be individually manned during Contractor’s working hours in order to ensure safety at each junction when one-way shuttle working is in operation.

Particular care must be taken to ensure that adequate forward visibility is always provided to 'STOP' points.

The Contractor should not limit the number of traffic management measures in operation on any one particular road at any one time. Successive delays greatly increase driver frustration and increase the risk of temporary controls being ignored which in turn risks impacting on the safety of road-users and construction personnel.

### 3.3.1 Traffic Control Methods for Shuttle Working

The traffic control methods to be utilised within the shuttle systems are outlined in Table 3.1, and shall be implemented in accordance with the relevant section of the Temporary Traffic Management Design Guidance to be referred to.

**Table 3.1: Traffic Control Methods**

Traffic Control Method	TTM Design Guidance 2019
Stop and Go disks	0.5.2.4
Temporary Traffic Signals	0.5.2.5
Convoy Working	0.5.2.8
All Stop	0.5.2.6

Lane widths are to be in accordance with Table 0.4.3.4.1 of Temporary Traffic Management Design Guidance, reproduced in Table 3.2.

**Table 3.2: Lane Widths**

	Normal traffic including buses and HGVs (m)	Cars and light vehicles only (m)
Two-Way working	6.75 desirable minimum 6.0 absolute minimum	5.5 desirable minimum 5.0 absolute minimum
Shuttle Working	4.3 maximum 3.3 optimum 3.0 absolute minimum	4.3 maximum 3.3 optimum 2.5 absolute minimum

### 3.4 Diversions and Alternative Routes

All diversions and alternative routes are to be carried out in accordance with Section 0.5.2.9 of the Temporary Traffic Management Design Guidance, published by the Department of Transport, Tourism and Sport 2019. Preliminary diversions and alternative routes for The Works are presented in Appendix A. Final diversions and alternative routes will be outlined in the Contractor’s Temporary Traffic Management Plan.

### **3.5 Road Closures**

Where necessary, all road closures are to be carried out in accordance with Section 0.2.5.4 of the Temporary Traffic Management Design Guidance, published by the Department of Transport, Tourism and Sport 2019.

While it is not envisaged that significant long-term road closures will be required to execute the works, off peak local road closures may be required where full carriageway paving coverage cannot be achieved through an alternate one-way shuttle working setup. The requirements of the Roads Act 1993, and Article 12 of the Road Regulations 1994 must be followed involving public advertisement and consultation.

### **3.6 Maintenance**

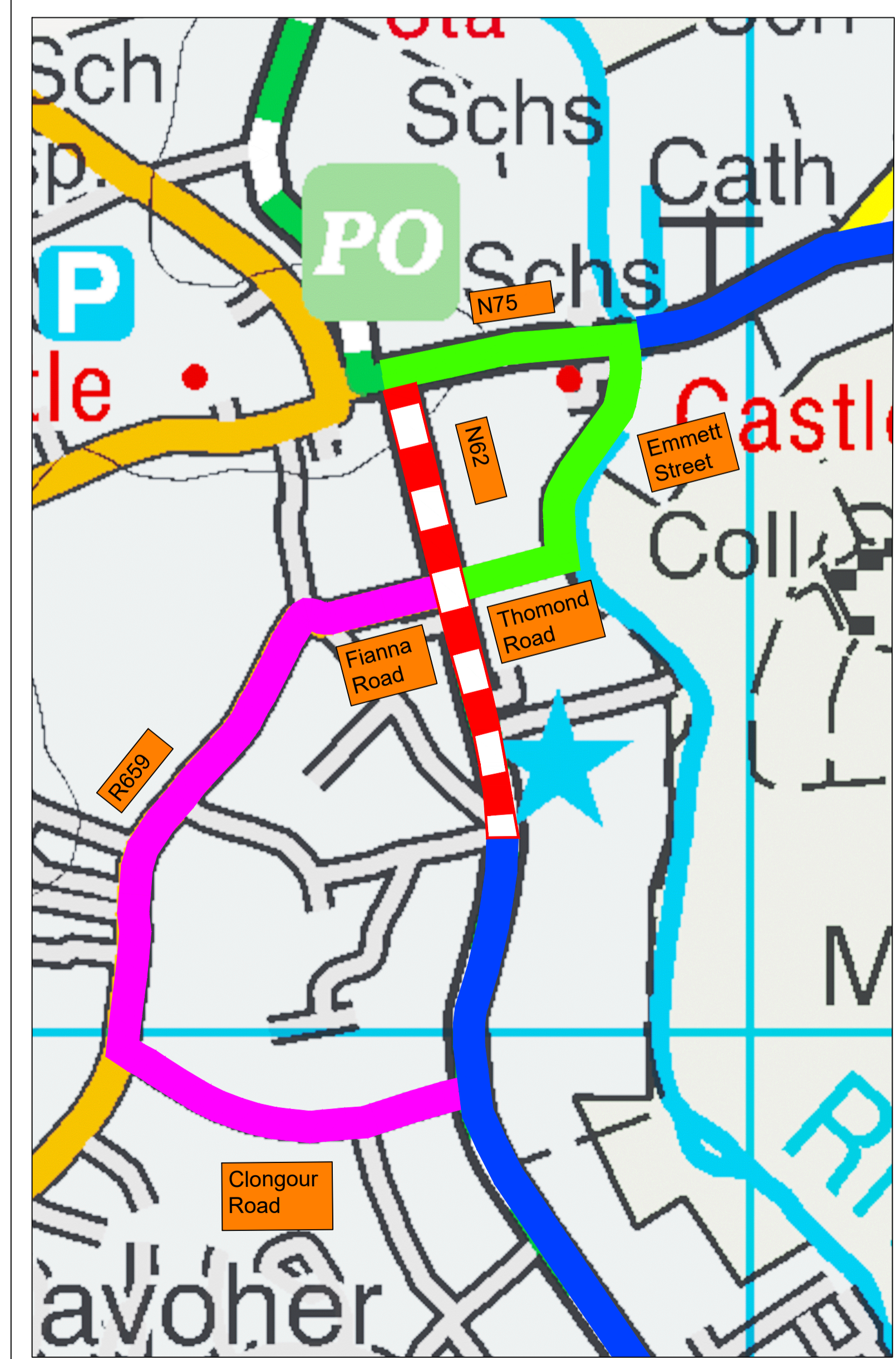
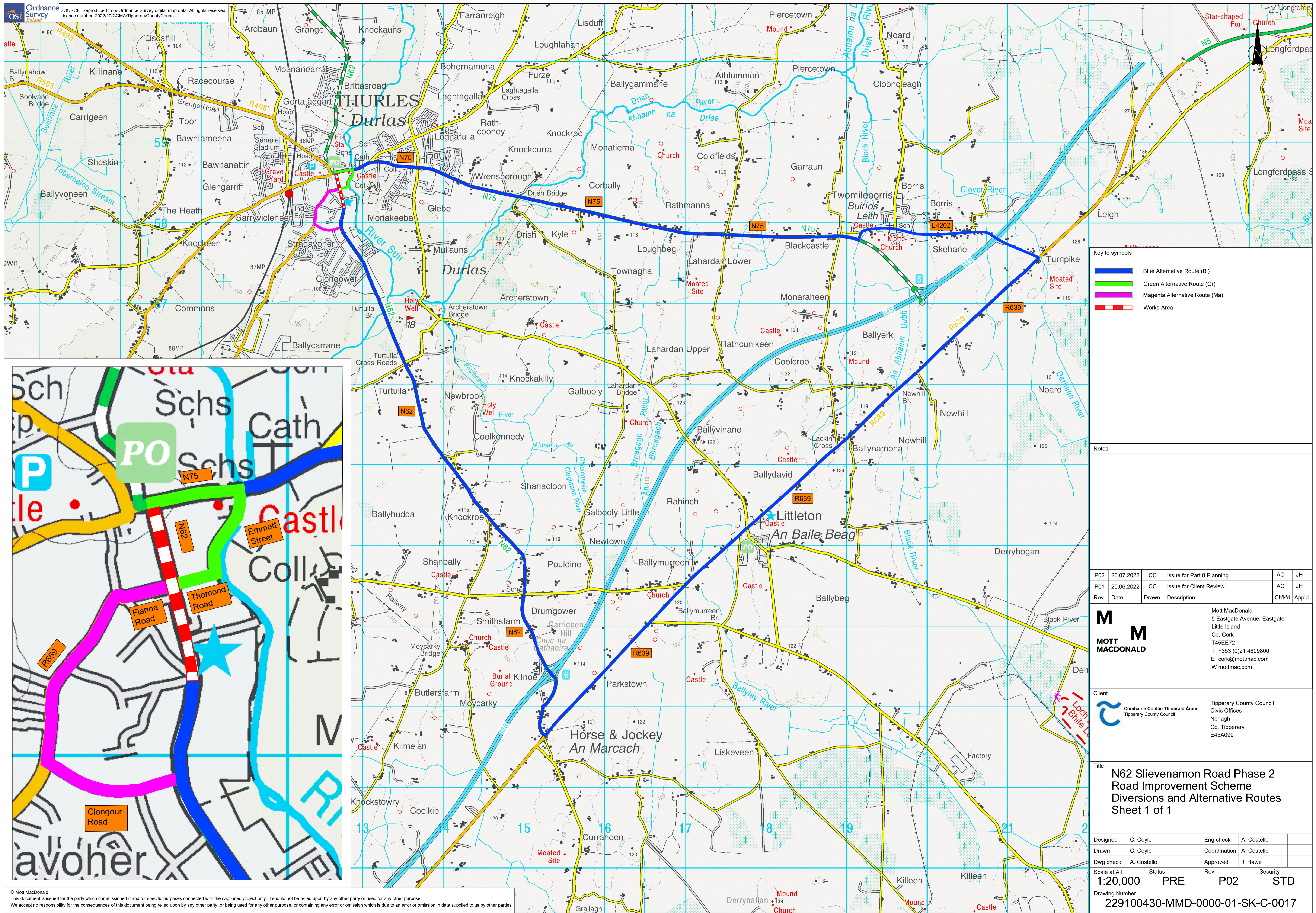
Provisions should be put in place by the Contractor to ensure that full time maintenance and patrolling of the traffic management system is provided at all times, at night, at weekends and at all times outside of the Contractor's working hours.

Where reinstatement of Traffic Management is necessary in case of an emergency the Contractor is to provide a list of names, addresses and telephone numbers of the Contractor's staff who are responsible for the organising of emergency work, which must be made available to Gardaí and the Roads Authority.



## A. Alternative Route and Diversions

### A.1 229100430-MMD-0000-01-SK-C-0017



Key to symbols

	Blue Alternative Route (BI)
	Green Alternative Route (Gr)
	Magenta Alternative Route (Ma)
	Works Area

Notes

P02	26.07.2022	CC	Issue for Part 8 Planning	AC	JH
P01	20.06.2022	CC	Issue for Client Review	AC	JH
Rev	Date	Drawn	Description	Ch'k'd	App'd

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Title

**N62 Slievenamon Road Phase 2  
 Road Improvement Scheme  
 Diversions and Alternative Routes  
 Sheet 1 of 1**

Designed	C. Coyle	Eng check	A. Costello
Drawn	C. Coyle	Coordination	A. Costello
Dwg check	A. Costello	Approved	J. Have
Scale at A1	Status	Rev	Security
1:20,000	PRE	P02	STD
Drawing Number	229100430-MMD-0000-01-SK-C-0017		

